

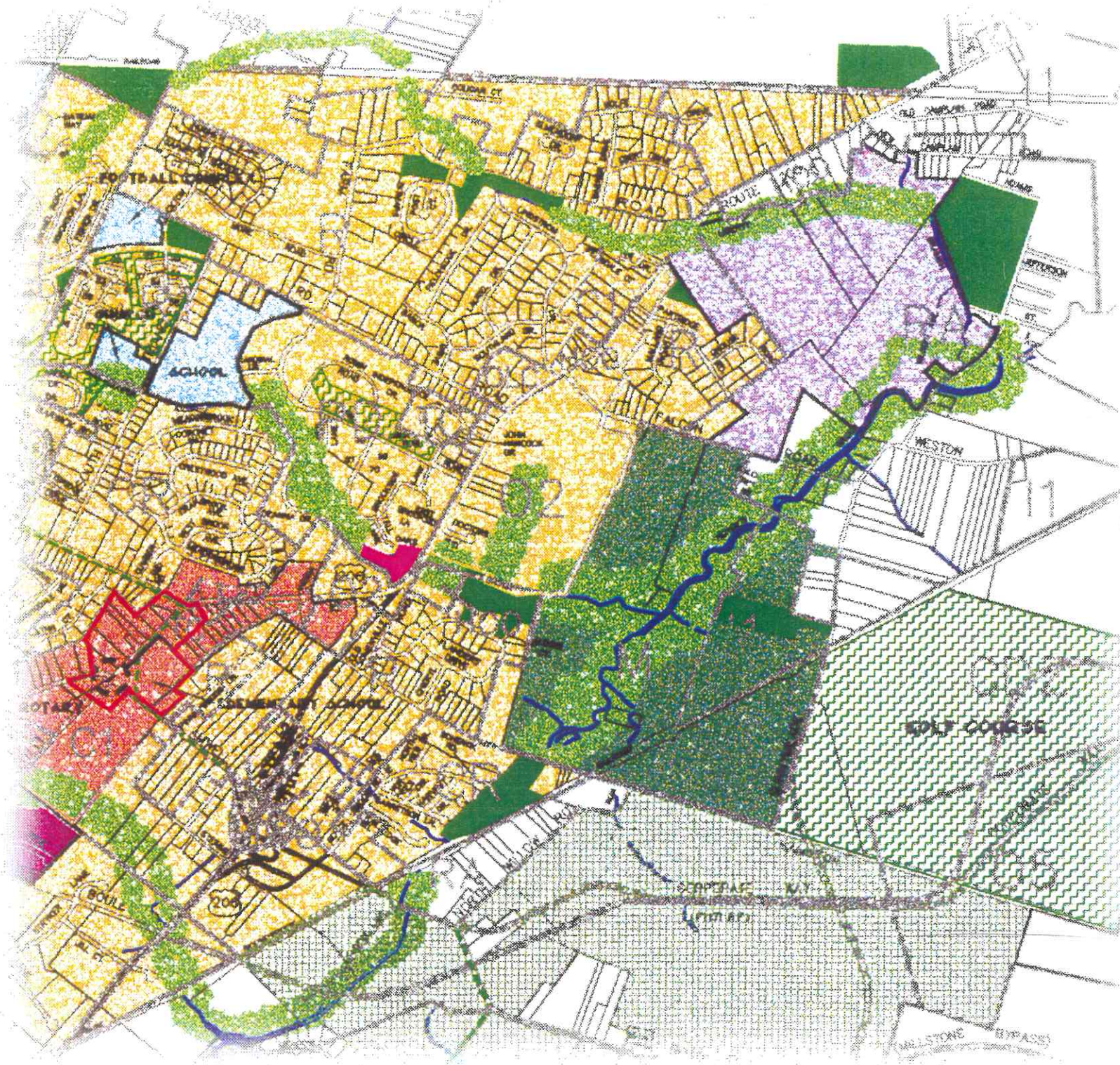
# MASTER PLAN AMENDMENT TOWN CENTER & MAIN STREET PLAN

TOWNSHIP OF HILLSBOROUGH,  
SOMERSET COUNTY, NEW JERSEY

PREPARED FOR THE TOWNSHIP OF  
HILLSBOROUGH PLANNING BOARD  
JUNE 1999

PREPARED BY:  
LOONEY  
RICKS  
KISS  
AND

**TM**  
ASSOCIATES





# MASTER PLAN AMENDMENT TOWN CENTER & MAIN STREET PLAN

Township of Hillsborough  
Somerset County, New Jersey

Prepared for  
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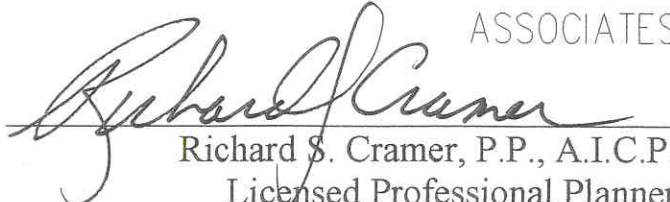
June 1999

Prepared By:

L O O N E Y  
R I C K S  
K I S S

and by

**TM**  
ASSOCIATES

  
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Licensed Professional Planner  
No. 002207

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*The original of this document has been signed and sealed in accordance with New Jersey Law*

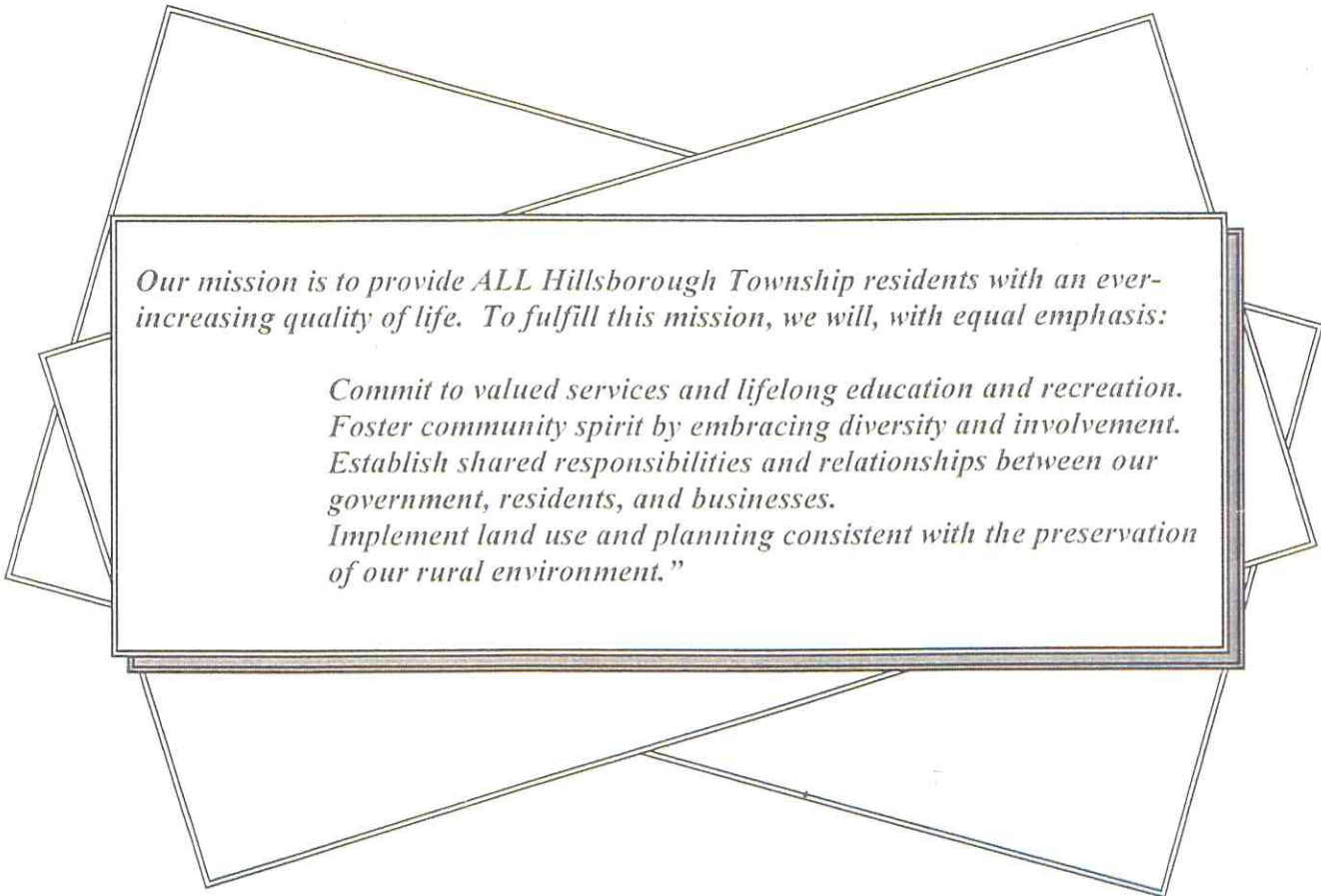
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# MAIN STREET PLAN

## Township of Hillsborough

Somerset County, New Jersey

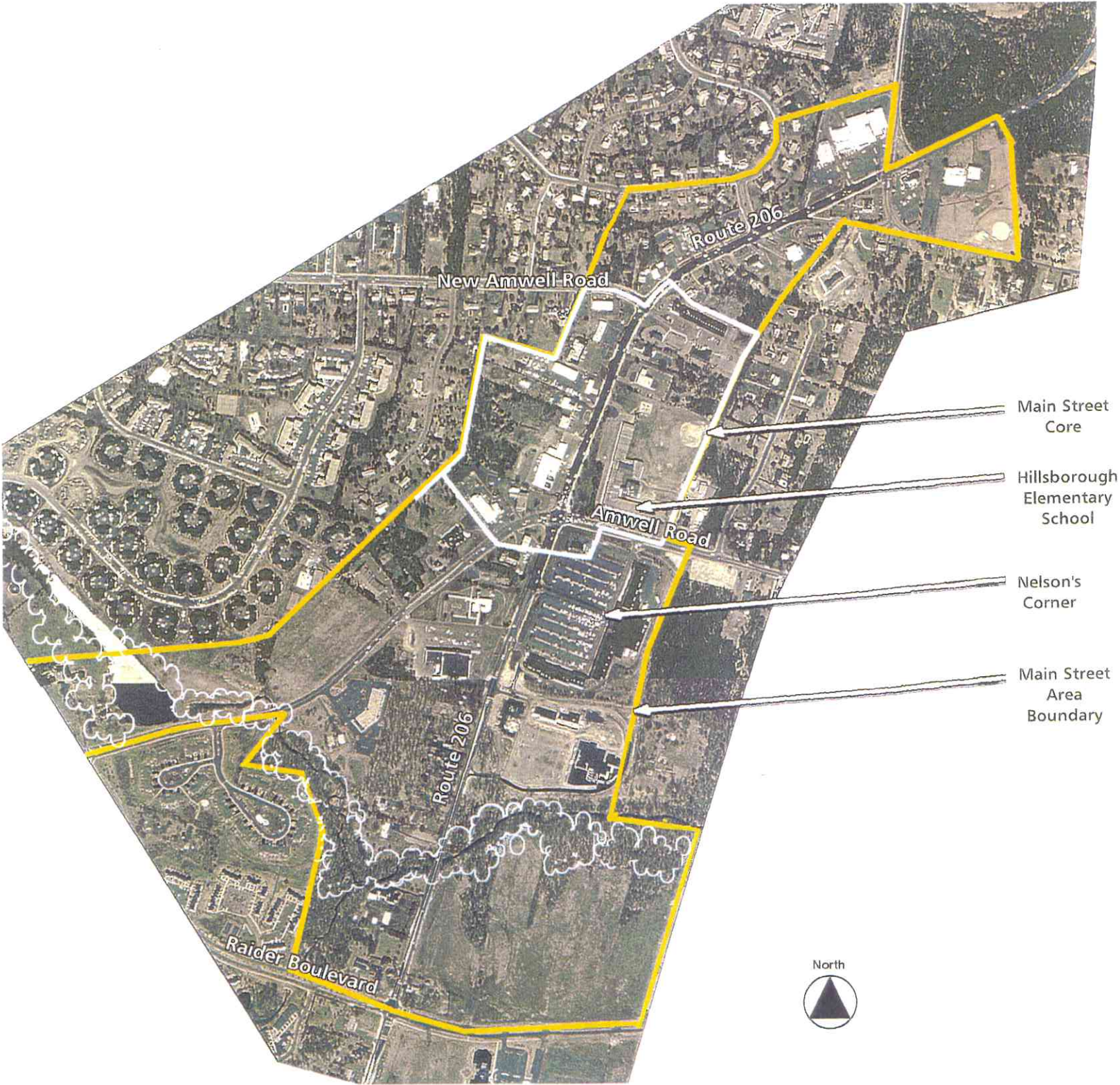
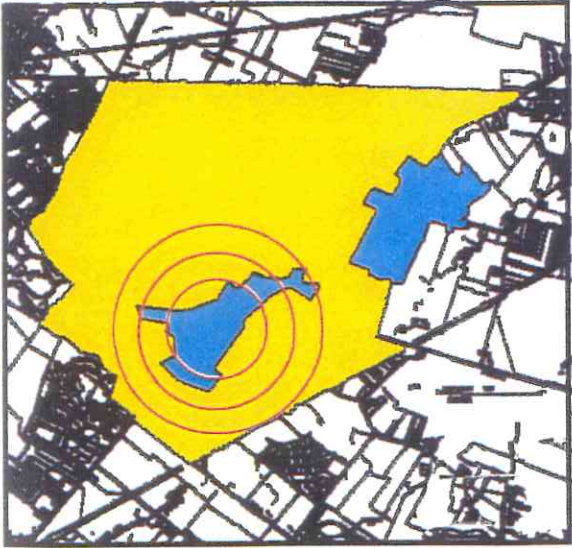


# BACKGROUND ISSUES

Hillsborough Township lies in an historic, rural and agricultural area of Central New Jersey that is now bisected by Route 206, a busy traffic corridor. Many citizens see commercial strip development as a cause for the town's "nondescript" appearance along Route 206. Given the strength of public opinion regarding the character and image of the Route 206 corridor, the Township's public outreach program utilized community preferences as the basis for designing a Master Plan to transform the area. Building upon this foundation of community input, the Master Plan responds to the opportunities for creating a Main Street Area/Town Center District that will provide a greater sense of community.

The 1994 Re-examination of the Master Plan expressed preferences for ease of circulation and improvements of infrastructure to accomodate future economic and residential development. This growth was coupled with a desire for the conservation of open spaces through the channeling of growth into a mixed-use core within a larger, clearly defined development area.

In 1997, a series of public planning workshops conducted by the Township reiterated the need for convenient circulation throughout the Township, with focused growth in order to preserve open space and the rural and agricultural character of Hillsborough. Such growth would permit a network of interconnected streets within the Township and would improve public and private sector services for Hillsborough citizens. The Township's prolonged outreach efforts to create coordinated, cohesive planning policies reinforced residents' desires for a stronger sense of community and identity.



# PLANNING PROCESS

**Community Participation**  
This study is intended to help guide on-going decisions by the Township, community organizations, businesses and private property owners. All of these groups share a common interest in enhancing the future viability of a Main Street Area for Hillsborough. Background issues and recommendations in this study stem from the input by Hillsborough residents as reflected in the Community Vision Survey and numerous public workshops attended by interested citizens, business officials and Township representatives.

**Community Vision Survey**  
In the summer of 1998, the Township organized a computer survey to which 2,277 participants responded. Results of the Community Vision Survey indicate that the majority of respondents were dissatisfied with existing conditions along the Route 206 corridor in the Main Street Area. Many felt that traffic was too heavy to make the area inviting. There was a sense of randomness and a general lack of cohesion and identity in the area.

Given such opportunities for change, the survey results showed a strong preference for a more pedestrian-friendly environment for Hillsborough's future Main Street. Responses emphasized the importance of respecting existing neighborhoods, while the creation of a variety of residential neighborhoods surrounding the Main Street Area would enhance the appeal of a downtown that reflected "the pulse of the community." The results of this survey are incorporated throughout this report as a reflection of community preferences that set the framework for this Plan.

**Community Workshops**  
Public workshops were used to help determine key issues for the Main Street Area. The workshop used discussion topics and the Community Vision Survey to explore overall issues and stimulate discussions on the future of the Main Street Area. Public outreach was targeted to key stakeholder groups, including businesses and property owners in the Main Street Area. The strongly felt desire for an attractive and inviting district with historic-like charm serves as a catalyst for change and as a vision for Hillsborough.

## Main Street Plan

Hillsborough Township, NJ		L O O N E Y	
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## THE VISION, GOALS AND OBJECTIVES

### A Vision for Hillsborough

Community input set the tone for future public improvements and development of the Main Street Area. The vision is for a safe, pedestrian-friendly, inviting and vital community center where residents and visitors can experience a strong sense of community. The areas depicted on the aerial photo (at left) provide for several exciting opportunities.

An essential change is the transformation of Route 206 from a suburban commercial strip corridor to a traditional Main Street Area. In addition to commercial and civic uses, this includes housing based on coherent design guidelines, as well as traffic calming devices. Public spaces with adjacent commercial areas including attractive commercial mixed use define the Main Street Area. The Main Street Area also would include a civic area as a focal point and a Town Green created on existing green space on the corner of Route 206 and Amwell Road.

### Goals and Objectives

The following goals and objectives have been identified for the Main Street Area:

- Establish a township focal point for employment, recreation, entertainment, commerce and government activities.
- Create an area with opportunities for working, shopping, and living all in one place.
- Create a pedestrian-oriented, multi-use center including plenty of trees, improved sidewalks, streets that "calm traffic", and pedestrian/bike paths which connect with surrounding neighborhoods.
- Provide options for walking, bicycling and mass transit as alternatives to automobiles.
- Promote street life along Main Street by including a selected amount of upper floor apartments and nearby housing which generate day and evening activity.

## Main Street Plan

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North





# MAIN STREET CORE

The core of the Main Street Area is approximately one-quarter mile in length along Route 206 from Amwell Road to New Amwell Road, comprising approximately 33.5 acres. The plan defines Main Street with a traffic rotary at the southern end and a new gateway building at the northern end. Main Street is divided into several traditional downtown blocks through the introduction of new streets, a Plaza and a Town Green. Existing vegetation, new buildings and other streetscape and landscape elements are used to reinforce the scaling-down of the existing automobile-scale highway corridor into the proposed pedestrian-scale downtown blocks.

Within the Main Street core, the new block pattern results in a series of new development opportunities. Traditional Main Street setbacks and other bulk and coverage standards allow for new buildings to be pedestrian-friendly while at the same time increasing visibility from passing vehicular traffic. This configuration will increase the number of corner sites as well as sites with frontage along the new cross streets and plaza that penetrate the properties along Main Street. This new pattern will create a host of new opportunities for property owners to better utilize their land.

**Main Street (Route 206)**  
Land uses in the Main Street Area will include a flexible mix of small to modest scale commercial uses, ranging from shops to restaurants to offices. Civic uses will also be permitted. It is recommended that a maximum of 100 upper floor apartments be permitted as bonus credits on a first-come, first-serve basis as an incentive to induce first phase development.

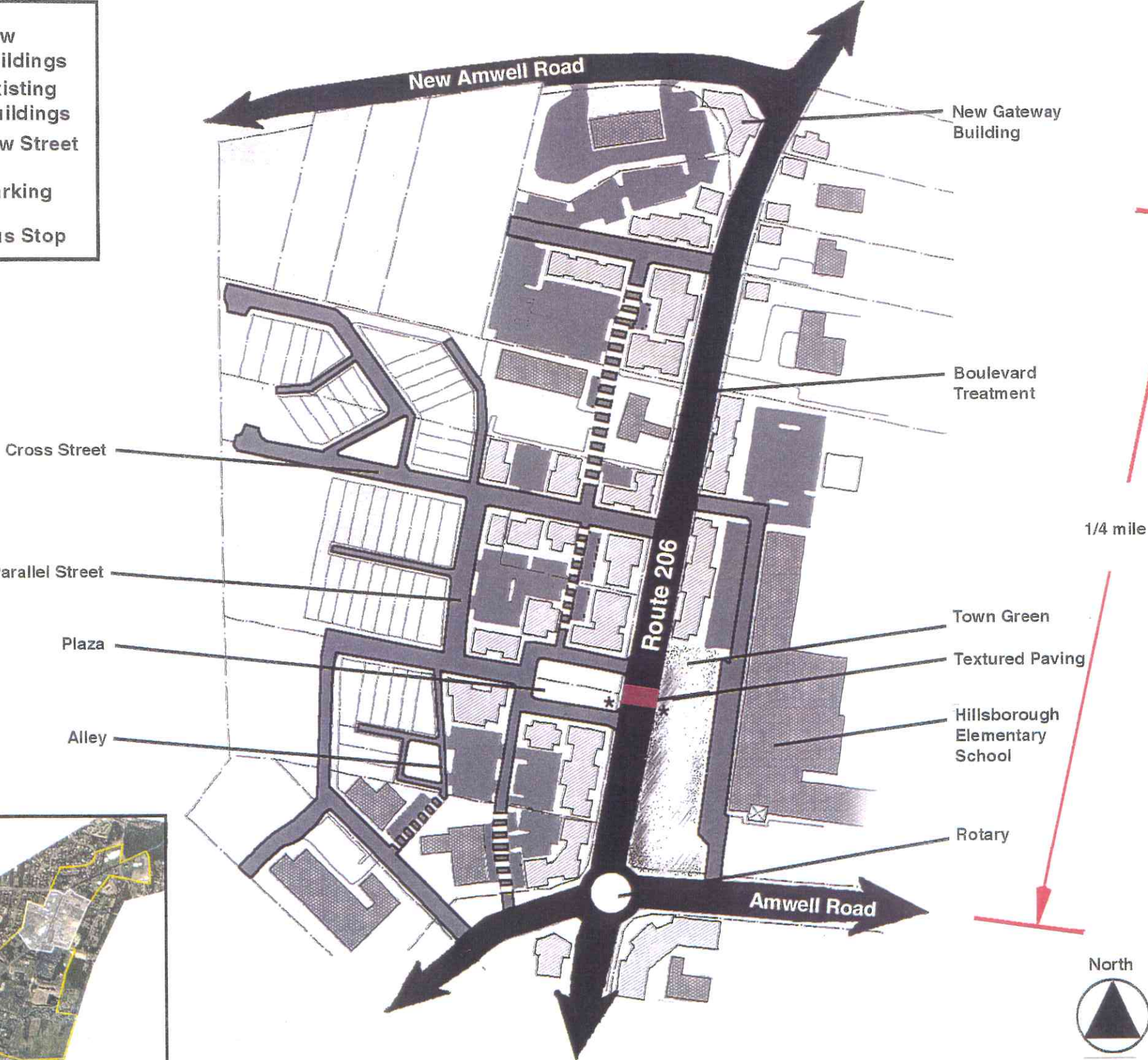
**Residential Neighborhood**  
A new residential neighborhood to be implemented only as the receiving zone or a non-contiguous residential cluster is located on the west side of Main Street as a transition area to the adjacent residential neighborhood. A mix of smaller, low-maintenance single family and townhomes is envisioned to provide an owner-occupied neighborhood within easy walking distance of Main Street. The homes should front onto streets and be served by rear access parking. Such development should conform with Town Center guidelines for residential development.

## Main Street Plan

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### LEGEND

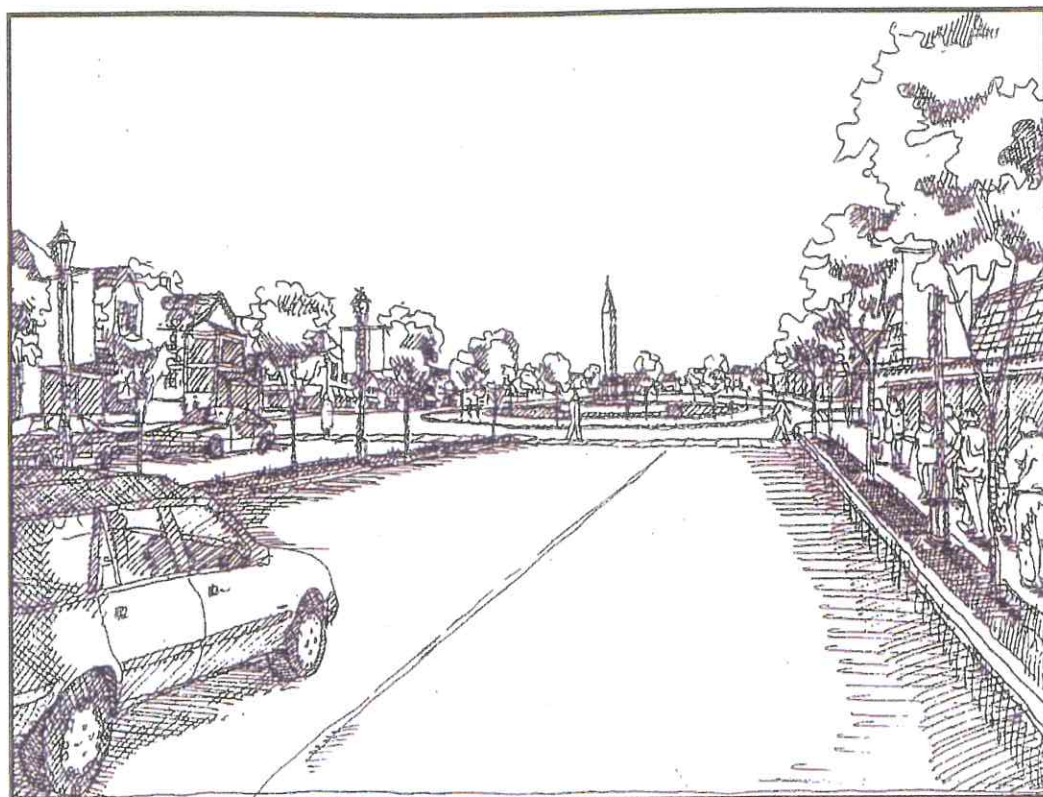
- New Buildings
- Existing Buildings
- New Street
- Parking
- Bus Stop



### KEY MAP

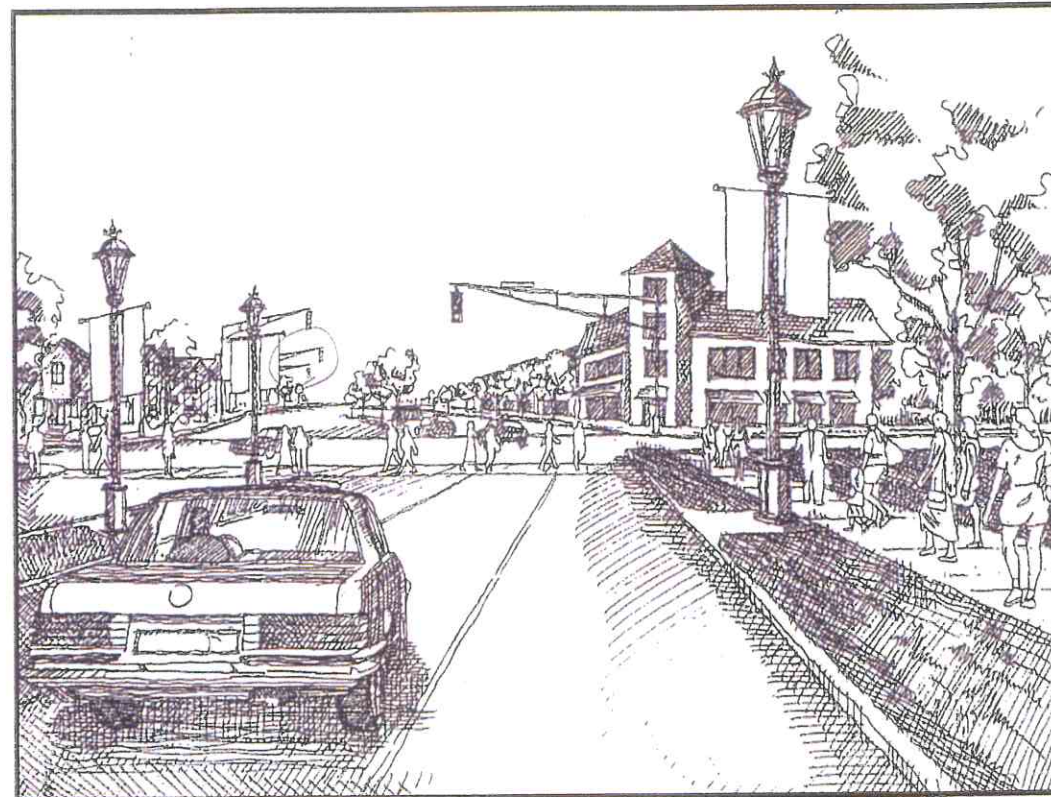
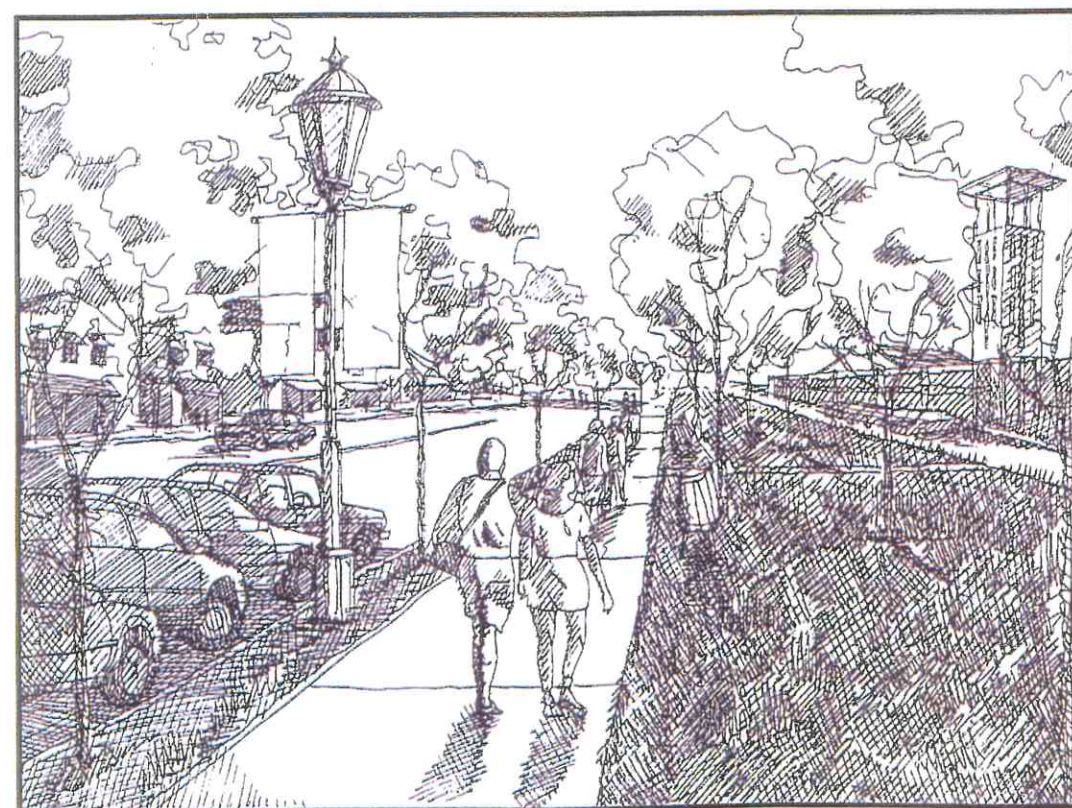






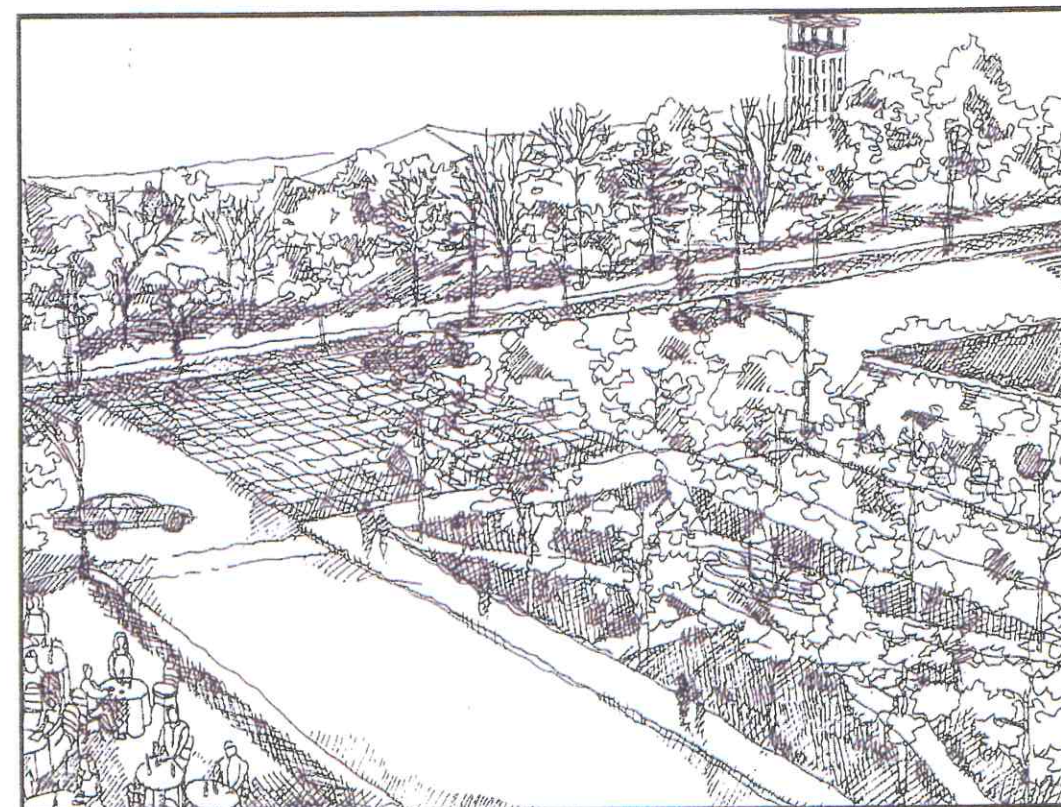
Approaching Main Street from the south, Route 206 is transformed into a green boulevard that intersects a rotary at Amwell Road. The rotary can serve as a civic focal point as well as an effective and safe replacement for a multi-lane traffic signal.

The lawn in front of the Hillsborough Elementary School will be transformed into a Town Green punctuated by a new tower that is reminiscent of a steeple.



Approaching Main Street from the north, a new gateway building at the corner of Amwell Road acts as a distinctive landmark. Shade trees, decorative lamp posts and banners further enhance awareness of entering the Main Street Core.

The plaza opens off Main Street across from the Town Green and is envisioned as a more active gathering space surrounded by shop fronts and cafes.



## MAIN STREET CORE

The Main Street Core is envisioned to have a series of visually interesting and walkable spaces, and highly visible and valuable real estate addresses for commercial uses. Public spaces are focused at the southern end of Main Street where the Town Green, plaza and rotary are located. The existing lawn in front of the Hillsborough Elementary School will need some re-grading and re-planting, new angled parking, walkways, benches and other improvements in order to be transformed into a Town Green. A new tower rising from the front entrance to the school is envisioned to serve as a civic focal point echoing the classic American townscape of steeples on greens.

The new public spaces will transform the current "non-descript" image rejected by citizens in the Community Vision Survey. In particular, these public spaces are intended to create the stronger sense of community that lay at the heart of citizens' vision for Hillsborough. While not intended as a pedestrian space, the rotary creates an ideal location for a civic focal point such as a statue or monument. The plaza is envisioned as a more active gathering space surrounded by shop fronts and cafes that enliven its edges.

The Main Street Core will derive much of its character from new buildings which frame streets as public spaces. Intimate-scaled buildings pulled up to sidewalks will make streets, as well as the Town Green, plaza and rotary, feel like a series of outdoor rooms enclosed by walls. This will stand in marked contrast to the existing condition of wide thoroughfares and low-scale buildings which sit in the midst of undefined spaces and parking lots. Benches, sidewalk cafes, tight spaced rows of shade trees and decorative streetlights and signage all reinforce the pedestrian-centered character.

## Main Street Plan

Hillsborough Township, NJ

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LOONEY

RICKS

KISS



# CIRCULATION

## Boulevards

The somewhat nondescript roads leading into the Main Street core including Route 206, Amwell Road, and New Amwell Road can be transformed into boulevards via the addition of central medians and extensive plantings and trees lining the roads. Motorist awareness of approaching the Main Street core will be enhanced when driving along green boulevards versus overly-wide roadways dominated by overhead utility wires, highway signage and striping.

## Cross and Parallel Streets

A major change to the core is the introduction of a highly interconnected street and pedestrian crossing system. New cross streets break up the length of Main Street and penetrate the depth of adjacent properties to the west of Route 206. A series of parallel streets, alleys and driveway/parking connections allow for various routes of pedestrian and vehicular movement between sites, similar to the recent development of Town Center Drive on the east side of Route 206. The location of the circulation system also responds to several utility easements running through the area. Overall the parallel and cross streets help create a circulation network that provides for access to parking lots, multiple connections and helps relieve the concentration of traffic and turning movements on Main Street.

## Utility Wires

Based on the results of the *Community Vision Survey*, the Plan envisions placing overhead utility wires underground to the extent possible. Removing the utility wires can help increase the safety of the Main Street Area. Tall overhead poles and wires do not visually cue passing motorists when they are driving through a low-speed, pedestrian-oriented Main Street Area.

## Transit Stops

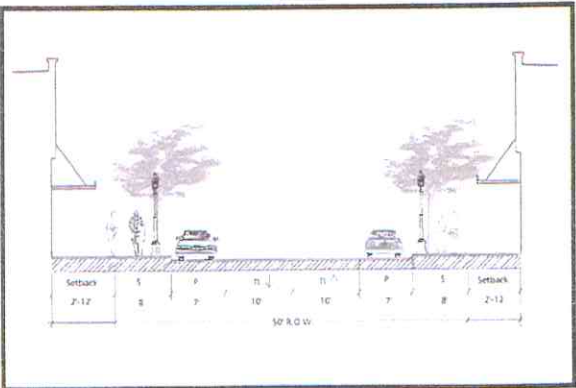
While no regular public transportation services the Main Street Area, future bus stops should be reserved across from one another at the Town Green and plaza.



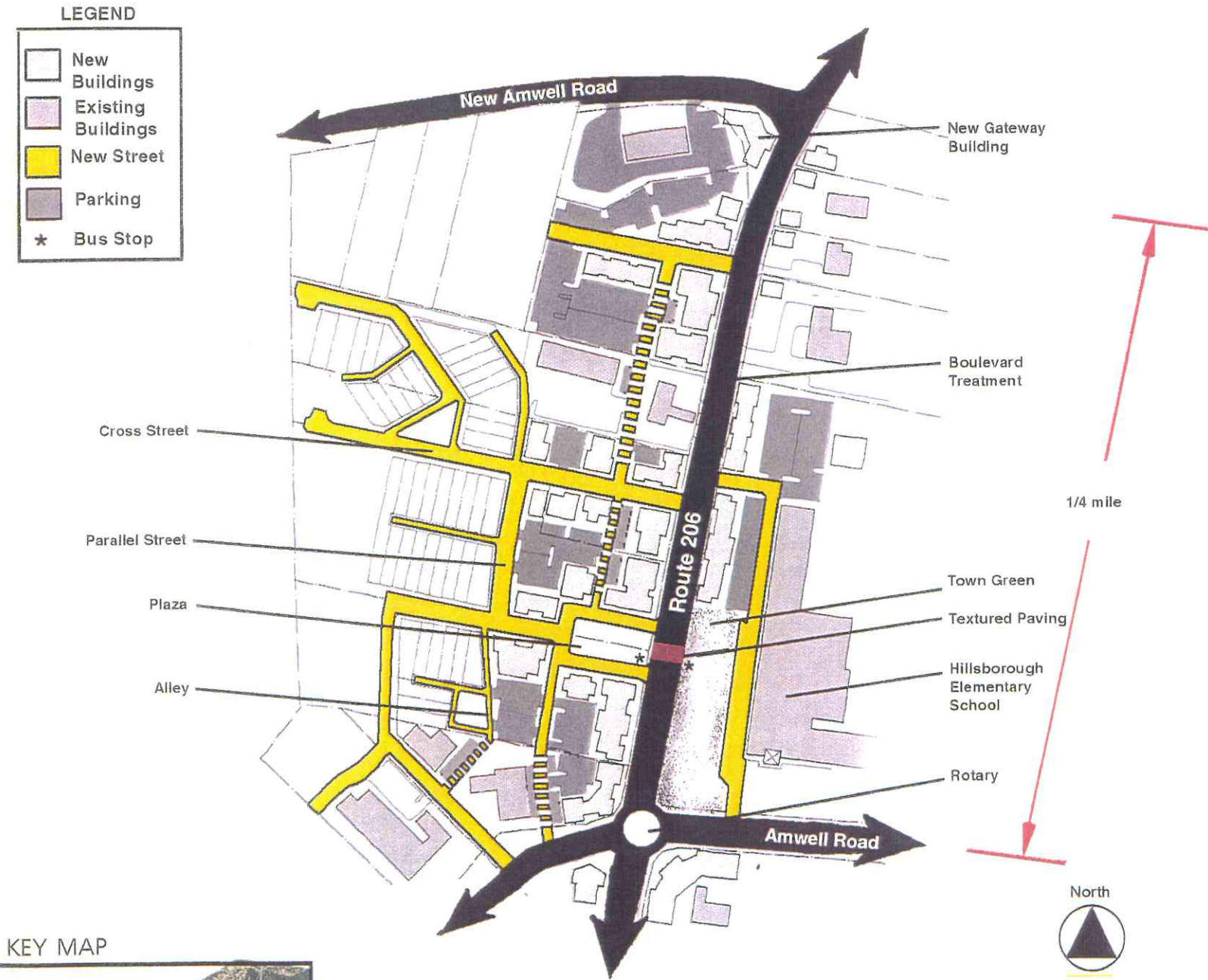
At the key location between the plaza and Town Green, street furniture, textured pavement and landscaping will help transform the street into a series of pedestrian-friendly public spaces.



Buried utility wires, tight rows of shade trees and decorative lampposts create pedestrian-friendly streetscapes.



New street cross-sections have been designed to calm traffic and accommodate on-street parking in balance with creating a safe and enjoyable pedestrian realm along adjacent sidewalks.



KEY MAP



## Main Street Plan

Hillsborough Township, NJ

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## CIRCULATION

### Sidewalks and Walkways

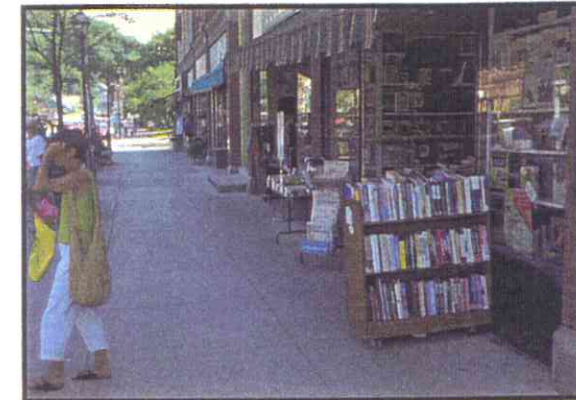
The entire Main Street Area should be served by pedestrian-friendly sidewalks and walkways that allow and encourage walking. An interconnected system of sidewalks and walkways should be provided along all streets and public spaces as well as throughout sites to connect all buildings with other buildings, parking areas, and public spaces. Walkways which are separate and distinct from motor vehicle circulation provide a pleasant route for users, promote longer stays for customers and encourage incidental social interaction among Township residents.

### On-Street Parking

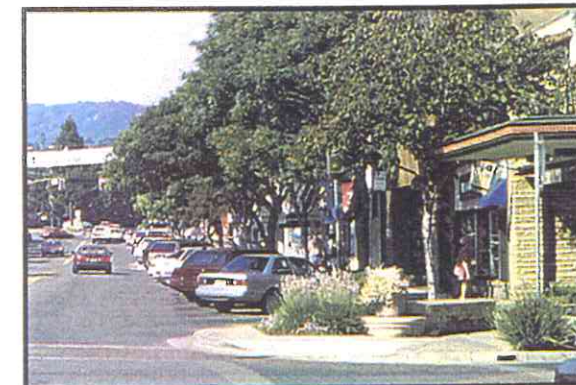
A major change from the existing development pattern is the provision of on-street parking. Approximately 241 new on-street spaces will be created by the Plan. Curbside parallel parking will line Main Street, cross and parallel streets and surround the proposed Plaza. These spaces will provide convenient customer access to new buildings located along these streets. Approximately 45 angled parking spaces will be provided surrounding the Town Green, providing an additional pool of new public parking. On-street parking spaces should be credited as a bonus for upper floor uses. Bonus credits for on-street parking spaces will be allocated on a first-come, first-serve basis as an incentive for development.

### Off-Street Parking

Off-street parking lots should only be accessed from cross and parallel streets and not directly from Main Street. Parking lots should be small-sized where possible, and should be interconnected with parking lots on adjacent properties where cross streets or parallel streets do not provide such interconnection. Cross-access easements permitting the interconnection of parking lots on adjacent lot should be required. Common, shared parking facilities are encouraged, where possible.



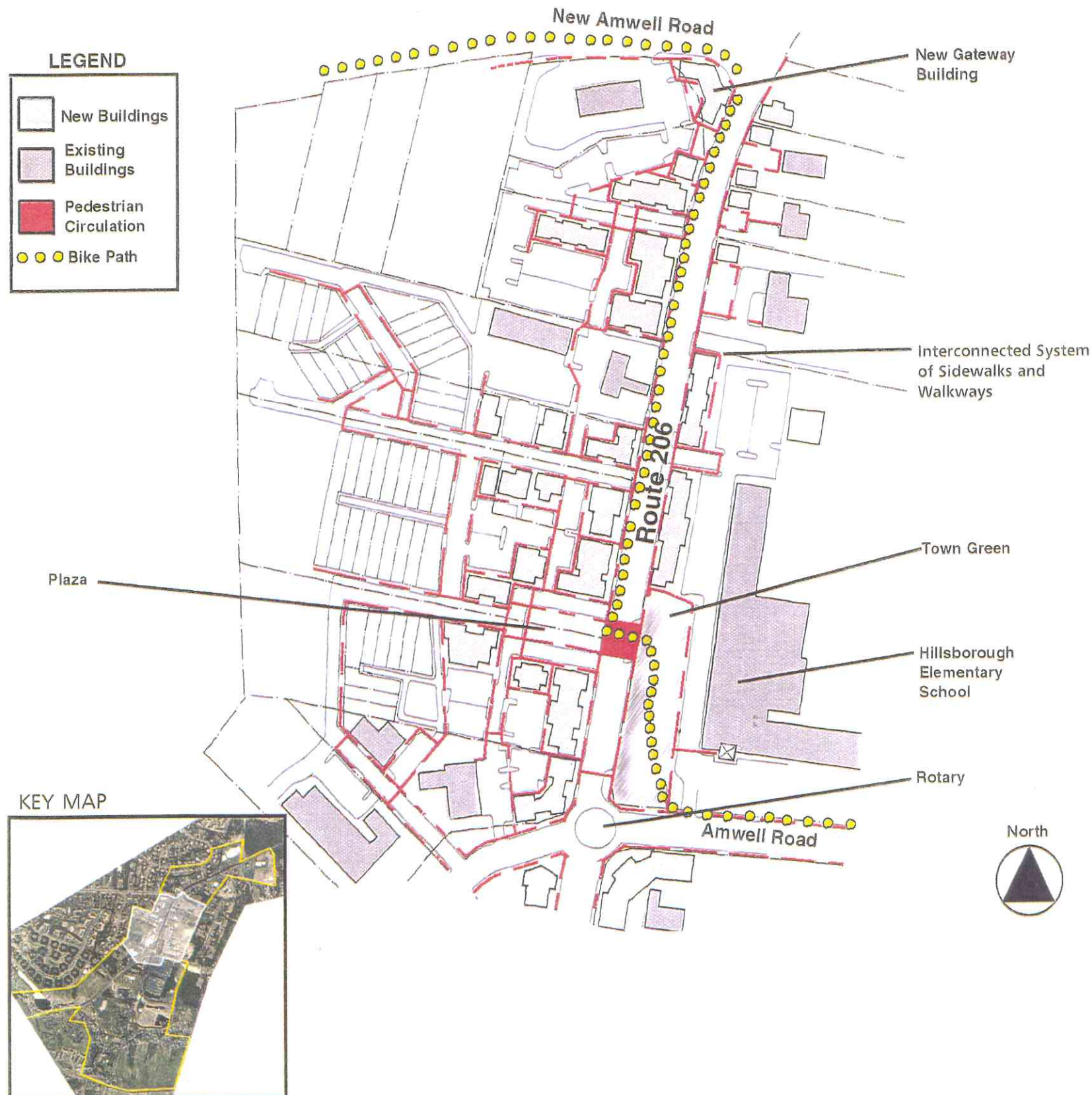
Pedestrian-friendly sidewalks create greater marketing opportunities for businesses while promoting social interaction which helps strengthen a sense of community.



On-street parking provides convenient customer access to all buildings in the core area.



Small-sized parking lots along cross, or parallel streets should be appropriately screened to reduce visual impact from the street.



## Main Street Plan

Hillsborough Township, NJ

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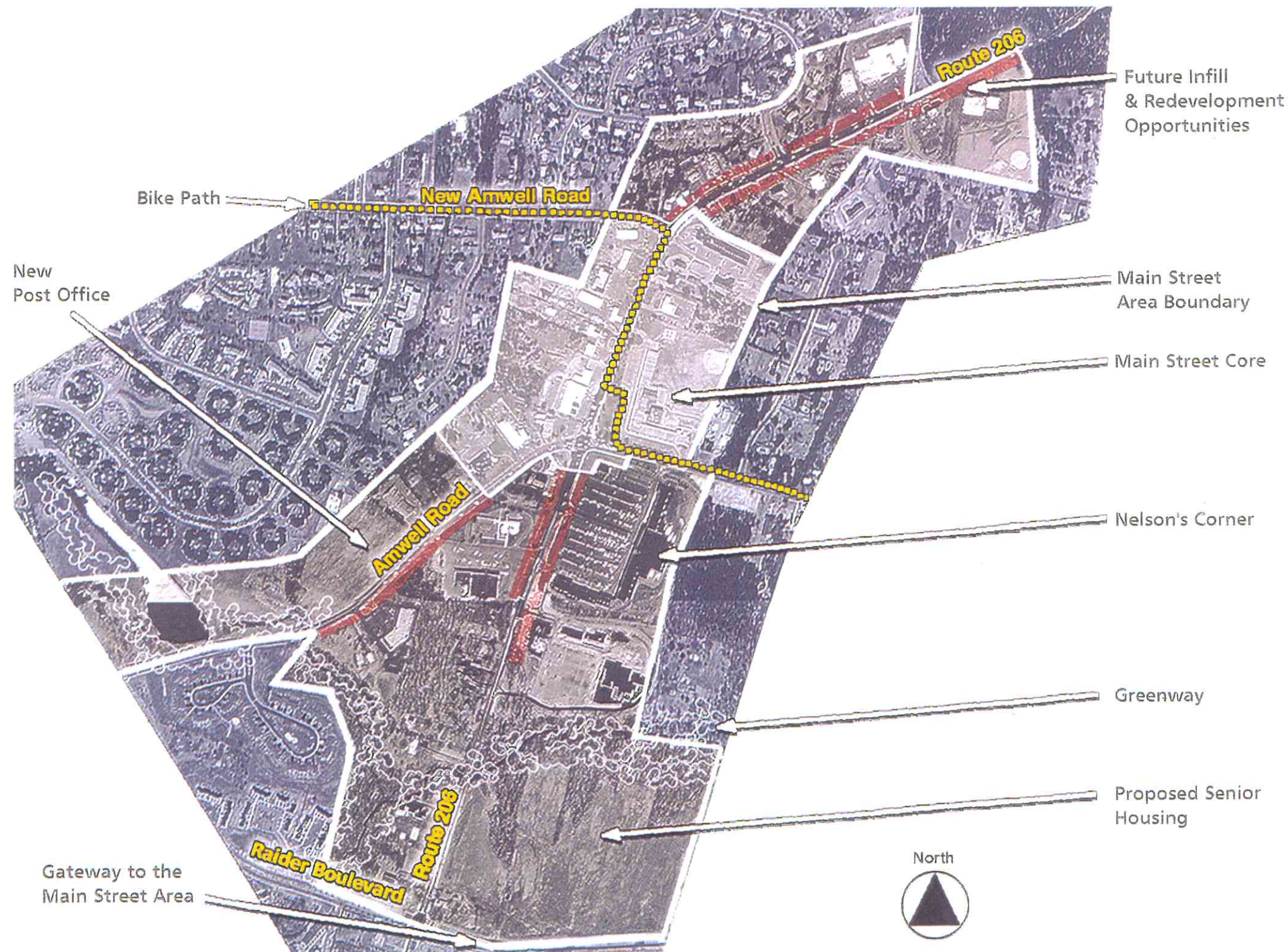


## BEYOND THE CORE

The Main Street Area extends beyond the core in all directions. No change is envisioned for existing residential neighborhoods within the Main Street area. However, further infill and redevelopment along Main Street can extend the core to the north and south. For instance, new infill buildings could be developed along Route 206 on land presently containing buffer strips and berms in front of Nelson's Corner and other shopping centers. This would allow existing commercial uses to integrate with Main Street and better utilize unproductive portions of existing sites. Gateway development at both of these northern and southern ends will help further define and enhance the extension of Main Street.

One of the most important civic uses that will be developed just outside the core will be the new post office along Amwell Road. An existing park and ride facility is also located in this area. New senior housing also is proposed for the southern end of the Main Street Area. Such development should conform with Town Center guidelines for residential development.

The Main Street Area is surrounded by the Town Center District which provides a surrounding residential population to support uses and activities. Traveling the Main Street Core is a bicycle route connecting to these surrounding Town Center Neighborhoods. Part of the townships greenway system winds along the southern portion of the Main Street Area and provides non-vehicular connection to surrounding neighborhoods.



## Main Street Plan

Hillsborough Township, NJ

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# TOWN CENTER DISTRICT PLAN

## Township of Hillsborough

Somerset County, New Jersey

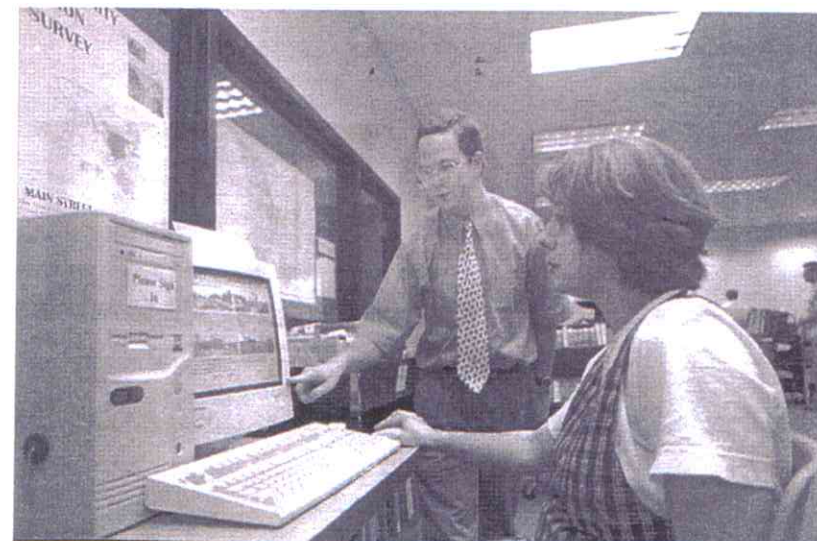
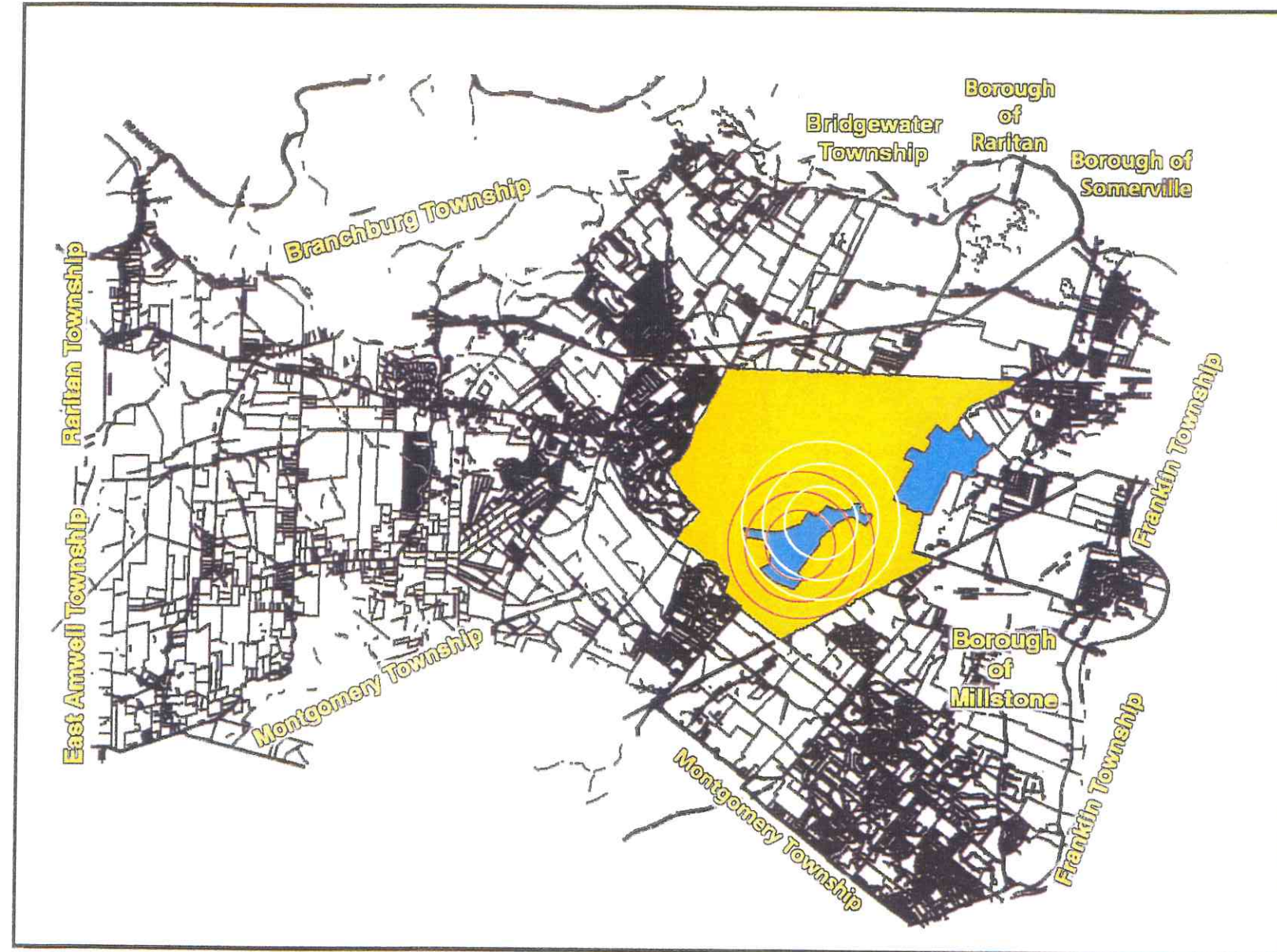


## BACKGROUND ISSUES

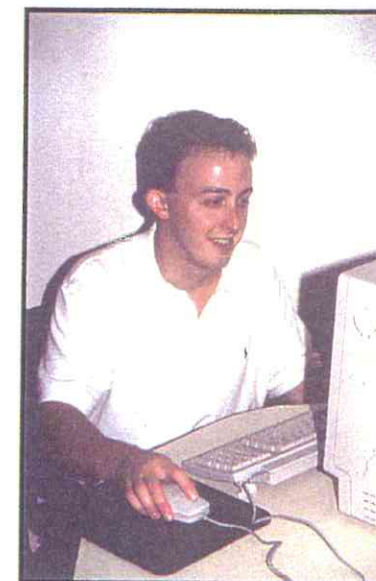
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RANDALL MILLER/STAFF PHOTOGRAPHER  
Geoffrey Urbanik shows Hillsborough resident Helen Homoki how the computer survey works at the Hillsborough Library.



2,277 people participated in survey.

## PLANNING PROCESS

### Community Participation

This study is intended to help guide on-going decisions by the Township, community organizations, businesses and private property owners. All of these groups share a common interest in enhancing the future viability of a Main Street Area/Town Center District for Hillsborough. Background issues and recommendations in this study stem from the input by Hillsborough residents as reflected in the Community Vision Survey and numerous public workshops attended by interested citizens, business officials and Township representatives.

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## Town Center Plan

Hillsborough Township, NJ

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Goal: Preserve Open Space Elsewhere by Concentrating Growth in the Town Center District.



Goal: Establish a Physical and Visual Identity for the Town Center District Within the Township



Goal: Promote Growth in the Town Center District Through Commercial & Residential Development



Goal: Create Civic and Open Spaces Adjacent to Commercial and Residential Development



Goal: Provide Easy and Safe Accessibility to the Nearby Main Street Area

## THE VISION, GOALS AND OBJECTIVES

### A Vision for Hillsborough

Community input set the tone for future public improvements of the Town Center District. During workshops and public meetings, Township residents, officials and interested developers expressed a desire for easily accessible employment opportunities and commercial services that were compatible with existing development. Pedestrian links to a variety of housing opportunities in new neighborhoods would act as connections and allow existing neighborhood to remain largely unchanged. Such changes in Hillsborough create a cohesive network of neighborhoods, civic and commercial services. The vision is for a safe, pedestrian friendly, inviting community with easy access to commercial and civic facilities. With the Main Street Area acting as a hub of denser development, the Town Center District functions as an expansion of employment and living opportunities while preserving open space and the rural character of outlying areas of the Township.

### Goals and Objectives

Goals and objectives are shown at left with accompanying images.

## Town Center Plan

Hillsborough Township, NJ

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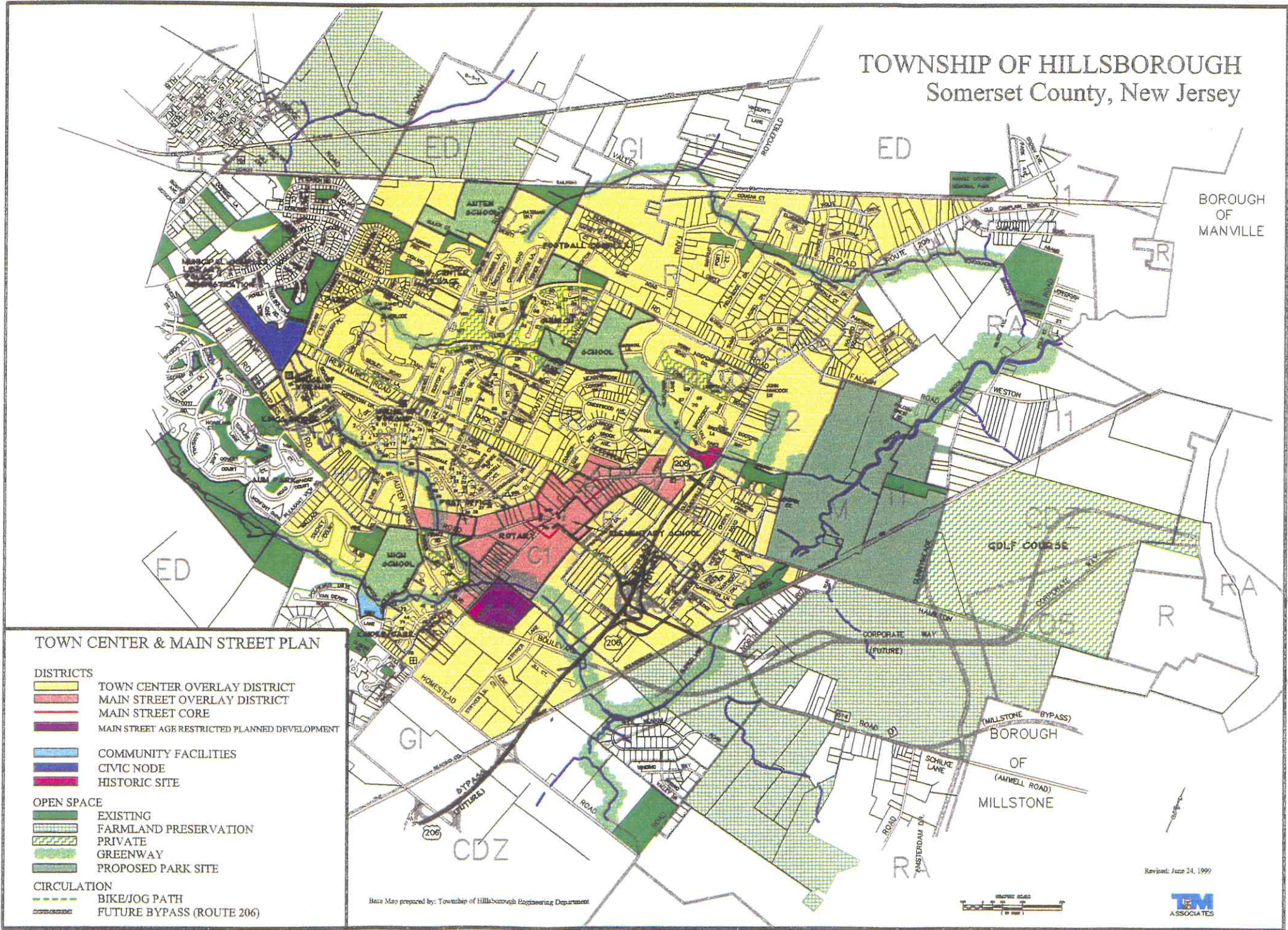
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TOWN CENTER PLAN

**The Town Center District**  
The Town Center District consists of 5.83 square miles bound by the Lehigh Valley Railroad line to the north, Route 206 and Boundbrook Road to the east, Homestead and Royce's Brook to the south. Beekman Lane to realigned Amwell Road (Route 514) is the westerly line. The Municipal Complex acts as a node at the western end of the Town Center District, bound by Pleasant View and Dunedain Road. Industrial complexes lie to the southeast in the Town Center District. Within the Town Center District, a greenway system of connected open space links different neighborhoods and the Main Street Area. Bikeways act as additional connectors between neighborhood nodes and the Main Street Area.

The Town Center is at the crossroads of State Road 206 & County Road 514 (Amwell Road), originally a stage-coach stop for the Pony Express. Historically, the Central Business District has evolved at the crossroads of the two roadways. Over the last twenty years planned unit development has occurred in the Town Center. Approximately 3,500 condominiums, townhouses, duplexes and apartments have been built in the Town Center. A substantial Industrial Park and retail uses are also in the Town Center.



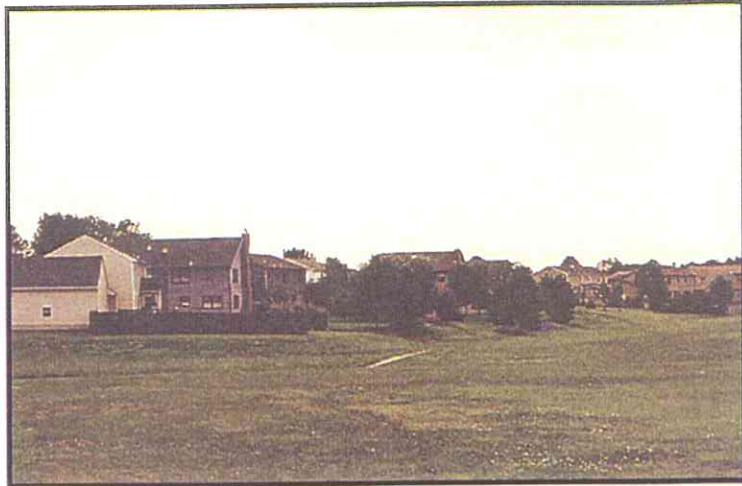
Town Center Plan

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Agricultural land and open space is preserved by the creation of the Town Center District.



A greenway system acts as a connector within the Town Center District.



The Lehigh Valley Railroad is a clear boundary at the northern edge of the Town Center District.



Easily accessible community facilities are located within the Town Center District. More such facilities should be added in the future.



Increased and safe pedestrian activity and bicycling is promoted in the Town Center District. New neighborhoods should incorporate pedestrian-friendly streetscapes.



New senior housing is targeted for the Town Center District.

# TOWN CENTER PLAN

The Town Center District encourages the agricultural preservation in the less accessible northern and western portions of the Township and thus protects operating farms and the area's rural character. There are three nodes within the Town Center; a retail node at the intersection of Routes 206 and 514, another retail node along Route 206, south of Falcon Road and north to Valley Road and a civic node at the most westerly boundary of the Town Center at Beekman Lane and County Road 514. Easily accessible commercial, civic and public facilities within walking distance of neighborhoods encourage activity within the district and reduce vehicular traffic and the perception of Hillsborough as a bedroom community.

There is a core of retail and office uses in the Town Center as well as an industrial base along Stryker Lane. Expanded employment opportunities within the Town Center District include a Home Occupation Office Zone which allows for work-at-home opportunities. Such options further reduce vehicular traffic by eliminating commuting to work.

The Town Center District responds to the realignment of Route 206 by creating a Main Street Core area as a focus for surrounding neighborhoods. The creation of this higher density core involves Transfer of Development Rights to preserve agricultural areas and farmland.

## Town Center Plan

Hillsborough Township, NJ

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## Statement of Relationship to the Existing and Proposed Zoning of the Township

Hillsborough Township covers a land area of almost fifty-six (56) square miles. Much of this area lacks sewer and water service and is environmentally constrained for development. The Town Center Plan and the Main Street Design Plan promote more efficient land use and prevent urban sprawl by directing future development and redevelopment into an environmentally suitable area of the community. The necessary infrastructure exists or will be provided to support future growth within a defined community development area. The available infrastructure within this community development area includes water and sewer service, schools, active recreation facilities, community facilities such as the library and the municipal administration and police building, places of worship, retail and service uses, and a network of roads constructed, or planned to be constructed, to provide capacity for growing traffic volumes.

The environs surrounding the community development area are largely rural. Open space preserves, such as the Sourland Mountain Preserve, farms, historic nodes, and environmentally sensitive features characterize the environs. A network of rural roads serves the environs. There is no water or sewer service and community facilities are limited. Along the Sourland ridge, well yields are deficient and the capacity of the area to provide a sufficient supply of potable water is extremely limited. The development of the environs is limited to low-density and low-intensity uses compatible with the rural character, environmentally sensitive features, and infrastructure constraints of the area.

To implement this plan amendment, three major overlay zone districts will be established. Two overlay districts will establish a development alternative by permitting the transfer of density from the rural environs of the Township into the community development area. The third overlay zone will permit

increased development intensity within a centrally located Main Street district. Additional overlays in the community development area will permit density transfers for the development of age restricted housing. Within the overlay zones, planned development based on the Town Center Plan and the Main Street Design Plan will be permitted as an additional use. The three major proposed overlay zone districts and their area are:

The Community Development and Town Center Overlay District  
21.65 Square Miles

The Open Space Conservation Overlay District  
23.57 Square Miles

The Main Street Overlay District  
0.35 Square Miles

The New Jersey Municipal Land Use Law enables municipalities to transfer density between noncontiguous tracts as a form of planned development. The overlay zone districts apply the State enabling legislation to manage future growth by transferring development to suitable areas while preserving open space and farms. The overlay districts will manage growth to achieve an overall vision that balances the development and conservation objectives of Hillsborough.

### *Community Development and Town Center Overlay District*

The Community Development Overlay District of the Township will be established as a zone to receive development density from the Open Space Conservation Overlay District. The underlying zoning will remain in place but, within the overlay districts, planned developed based on noncontiguous residential parcel clustering will be permitted. Residential clustering providing a required amount of open space will be allowed on noncontiguous

tracts that meet requirements as to size and location. The residential dwellings will be developed on tracts within the Community Development and Town Center Overlay District. The required open space will be located on tracts in the Open Space Conservation Overlay District. The tracts may be miles apart. The Community Development and Town Center Overlay District will permit the underlying residential zones, including the zones of the Town Center Plan and designated locations of the Main Street Plan, to serve as receiving areas for the transfer of residential development from the environs. The open space parcels to be preserved will be located in the environs. Minimum and maximum requirements will be established for the parcel to be built upon.

### *Open Space Conservation Overlay District*

The additional density in the Community Development and Town Center Overlay District will be transferred from lands currently zoned for residential use in the environs of the Township. To implement the transfer, a separate zone overlay district for open space conservation outside the community development boundary will be established. The Open Space Conservation Overlay District will permit the transfer of density from the underlying residential zones into the community development area of the Township as part of a noncontiguous planned development. The land from which the density is transferred will be preserved as the open space component of the noncontiguous planned development.

Noncontiguous clustering will also be permitted conditionally between tracts that are located within the South Branch area of the Conservation Overlay District. This area, generally recommended for designation as State Planning Area 4, Rural, contains farms and open space that merit preservation. Allowing intra-district transfers in this area will increase the opportunities to



preserve farmland. The maximum density for an intra-district receiving tract should be set based upon the availability of the sewer and water infrastructure and the underlying zoning. A large area of open space must be preserved within the South Branch area as part of the intra-district transfer.

Minimum requirements will be established to qualify a parcel for conservation purposes as part of a noncontiguous planned development.

*Main Street Overlay District*

At the center of the community development area, the Main Street Design Plan establishes an additional overlay zone for a mixed-use core to promote a pedestrian environment, shared parking, and a concentration of retail services, personal services, community facilities, and civic open space. The underlying zone designations are unchanged. However, tracts within the Main Street Overlay District will be permitted to redevelop as a planned commercial development in accordance with the Main Street Design Plan. A Main Street Planned Commercial Development will be permitted a wider range of uses at greater intensity than currently allowed by the underlying zone. The design and layout of a Main Street Planned Commercial Development must adhere to the Main Street Design Plan. Landscaping and plazas will be required as part of the development transfer to offset the intensity of development and create an attractive Main Street.

Up to one hundred (100) one (1) and two (2) bed room dwelling units will be permitted within the Main Street Overlay District as mixed use development. These residential units must be located above the first floor. As an additional incentive to develop Main Street, no density transfer will be required for mixed-use development.

*Standards for Density Transfer for Non-contiguous Planned Development*

The Town Center ringing Main Street, and the community development area outside the Town Center, contains existing residential development and community facilities. Residential density transfers will be permitted onto vacant tracts subject to standards for maximum density, minimum tract area, open space, and housing type. Unless otherwise specified on the Town Center Plan, or specifically designated for age-restricted development, only the housing types that are permitted by the underlying zone will be permitted as part of a non-contiguous residential cluster. If the underlying zone only permits single-family detached housing, then only single-family detached housing can be constructed at an increased density.

The maximum densities for non age restricted development that would be permitted in the community development area with a density transfer would be:

Underlying Zone	Maximum Density with Transfer (Dwelling Units per Acre)
AG	0.75
RA	1.5
R	3.0
TC	6.0
TC	12.0 (garden apartments only)

The maximum density permitted on a receiving tract as part of an intra-district transfer in the Conservation Overlay District in South Branch should be set with consideration to the underlying zoning and the availability of the sewer and water infrastructure to the receiving tract. The recommended density range is 0.75 to 2.0 dwelling units per acre.

The schedule of area, yard, and bulk standards for the noncontiguous planned development will be based upon the increased densities and the dwelling type.

*Standards for Density Transfer for Age Restricted Non-contiguous Planned Development*

Density transfers into specified locations within the Community Development and Town Center Overlay District Zone will be permitted for noncontiguous age restricted housing development. Two age restricted housing locations have been designated that abut or are near to the Town Center, and one location abuts the Main Street. The area of the designated locations are:

Age Restricted Planned Development  
0.10 Square Miles

Main Street Age Restricted Planned Development  
0.03 Square Miles

The maximum densities that would be permitted with a transfer as part of an age restricted planned development would be six (6) dwelling units per acre for housing types other than multi-family. Multi-family age restricted housing could be developed at density of up to twelve (12) dwelling units per acre. Assisted living facilities could be developed at a density of up to fifteen (15) dwelling units per acre.

The zoning schedule of area, yard, and bulk standards for the age restricted noncontiguous planned development will be based upon the increased densities and the dwelling type.



*Design Requirement for Main Street Planned Commercial Development*

To ensure that the vision for Main Street is realized, the zoning should require that the design and layout and location of uses of a Main Street Planned Commercial Development be arranged to implement the Town Center and Main Street Plan of the Township Master Plan.

*Affordable Housing Requirement*

To equitably spread the future burden of providing affordable housing, the Township zoning regulations should require that all non-age restricted development for ten (10) or more new residential dwelling units, except for residential dwellings located above the first floor as part of a mixed-use Main Street Planned Development, must provide for affordable housing through a ten per cent (10%) set aside for actual construction or through an in-lieu contribution at the rate of \$20,000 per affordable unit.

The zoning regulations should further require that all age-restricted planned development must provide for affordable housing through a ten per cent (10%) set aside for actual construction or through an in-lieu contribution at the rate of \$20,000 per affordable unit.

Noncontiguous age restricted housing development for units other than assisted living, should be permitted an increment of density increase without being subject to further density transfers or set aside requirements, provided an affordable housing fee equal to six (6) per cent of the value of the units realized from the increase is paid into the affordable housing trust fund of the

Township. The maximum density that could be realized for a non-contiguous age restricted planned development with an incremental density increase would be:

Type of Dwelling	Maximum Density (Dwelling Units Per Acre)
Multi-family	14
Other Dwelling Types	6.5

*Statement of Relationship to other Master Plan Elements*

The Town Center Plan and the Main Street Design Plan amend the goals and objectives, policies, and the land use element of the Master Plan of Hillsborough Township and its constituent elements for circulation, utilities, community facilities, recreation, conservation, economic development, and historic preservation. If any component of the Master Plan as previously adopted conflicts with the Town Center Plan and Main Street Design Plan, then the Town Center Plan and Main Street Design Plan shall govern, except that no change is proposed to the specific compliance measures adopted as part of the housing plan element and fair share plan for providing for the Township fair share obligation for the lower income housing need for the period from 1987-1999.

*Statement of Plan Relationships*

The amendment to the Township Master Plan for the Town Center Plan and the Main Street Design Plan is compatible with the development of

neighboring municipalities, the Somerset County Plan, the County Solid Waste Management Plan, and the State Development and Redevelopment Plan.

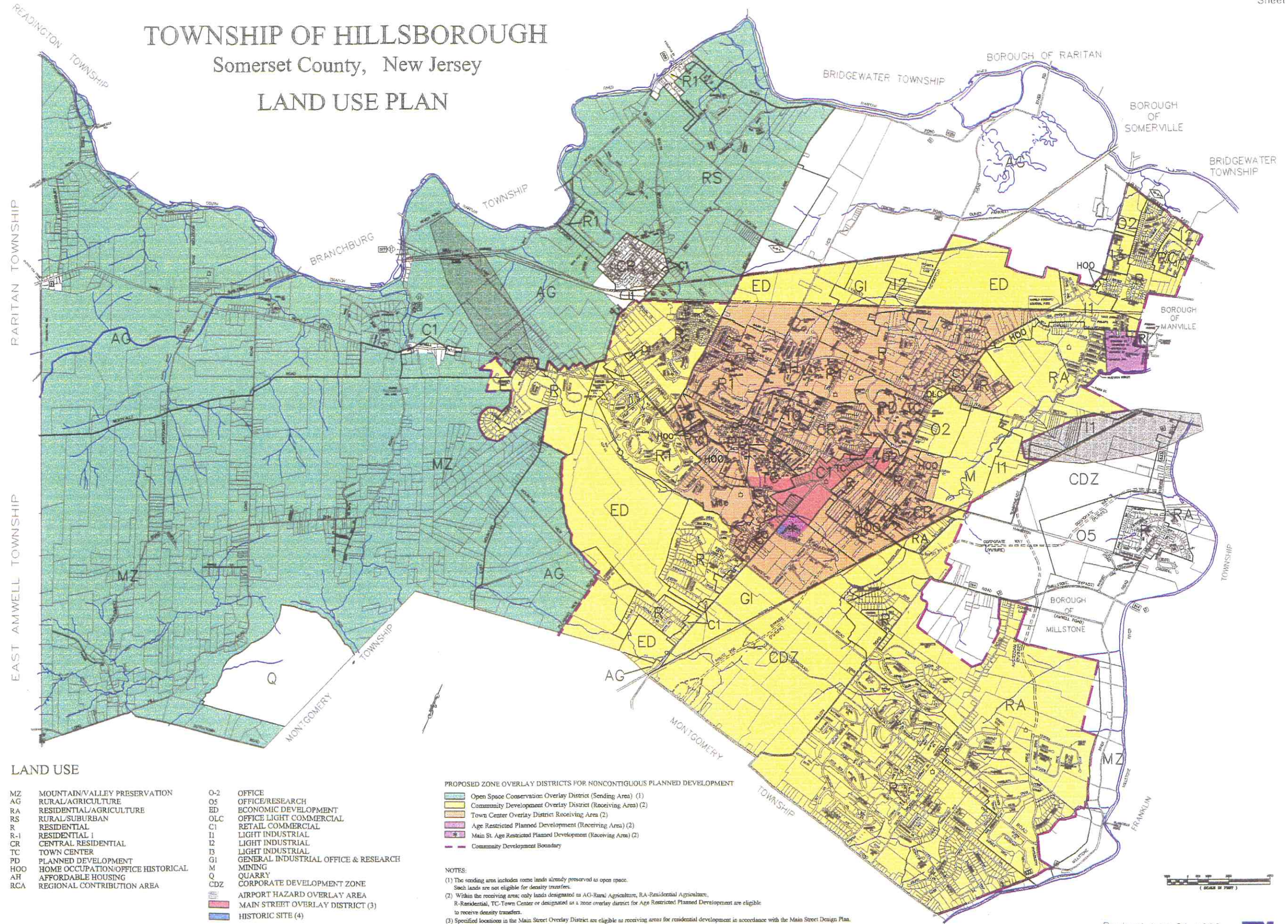
This amendment to the Township Master Plan was prepared during the cross-acceptance process for the revised State Development and Redevelopment Plan and is based upon a general reexamination of the Township Master Plan which has included a review of the plans of the neighboring municipalities and the County plan. The Township has coordinated its planning effort with the proposed policy area designations of the State, the County, and neighboring municipalities and has petitioned the State for Town Center designation pursuant to the State planning process.



# TOWNSHIP OF HILLSBOROUGH

Somerset County, New Jersey

## LAND USE PLAN



### LAND USE

MZ MOUNTAIN/VALLEY PRESERVATION  
 AG RURAL/AGRICULTURE  
 RA RESIDENTIAL/AGRICULTURE  
 RS RURAL/SUBURBAN  
 R-1 RESIDENTIAL  
 CR CENTRAL RESIDENTIAL  
 TC TOWN CENTER  
 PD PLANNED DEVELOPMENT  
 HOO HOME OCCUPATION/OFFICE HISTORICAL  
 AH AFFORDABLE HOUSING  
 RCA REGIONAL CONTRIBUTION AREA

O-2 OFFICE  
 O-5 OFFICE/RESEARCH  
 ED ECONOMIC DEVELOPMENT  
 OLC OFFICE LIGHT COMMERCIAL  
 C1 RETAIL COMMERCIAL  
 I1 LIGHT INDUSTRIAL  
 I2 LIGHT INDUSTRIAL  
 I3 LIGHT INDUSTRIAL  
 G1 GENERAL INDUSTRIAL OFFICE & RESEARCH  
 M MINING  
 Q QUARRY  
 CDZ CORPORATE DEVELOPMENT ZONE  
 AIRPORT HAZARD OVERLAY AREA  
 MAIN STREET OVERLAY DISTRICT (3)  
 HISTORIC SITE (4)

### PROPOSED ZONE OVERLAY DISTRICTS FOR NONCONTIGUOUS PLANNED DEVELOPMENT

- Open Space Conservation Overlay District (Sending Area) (1)
- Community Development Overlay District (Receiving Area) (2)
- Town Center Overlay District Receiving Area (2)
- Age Restricted Planned Development (Receiving Area) (2)
- Main St. Age Restricted Planned Development (Receiving Area) (2)
- Community Development Boundary

### NOTES:

- (1) The sending area includes some lands already preserved as open space. Such lands are not eligible for density transfers.
- (2) Within the receiving area; only lands designated as AG-Rural Agriculture, RA-Residential Agriculture, R-Residential, TC-Town Center or designated as a zone overlay district for Age Restricted Planned Development are eligible to receive density transfers.
- (3) Specified locations in the Main Street Overlay District are eligible as receiving areas for residential development in accordance with the Main Street Design Plan.
- (4) Historic sites and land within the HOO district are not eligible as receiving areas.